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**VILLAGE OF BEAVERDAM**  
**Americans with Disabilities Act Transition Plan:**  
**Pedestrian Facilities in the Public Rights-of-Way**



**101 W. Main St.**  
**Beaverdam, OH 45808**  
**(419) 643-4231**  
**[www.beaverdamoh.com](http://www.beaverdamoh.com)**

**December 2018**





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## **INTRODUCTION**

The purpose of this Plan is to ensure that the Village of Beaverdam creates reasonable, accessible paths of travel in the public rights-of-way for everyone, including people with disabilities. The Village of



Beaverdam has made a significant and long-term commitment to improve the accessibility of their pedestrian facilities located within public rights-of-ways. However, gaps and barriers still exist. The Transition Plan identifies such physical barriers and prioritizes improvements that should be made throughout the Village of Beaverdam. This Transition Plan describes existing policies and programs to enhance the overall pedestrian accessibility.

## **LEGAL REQUIREMENTS**

The federal legislation known as the Americans with Disabilities Act (ADA), enacted on July 26, 1990, provides comprehensive civil rights protections to persons with disabilities in the areas of employment, state and local government services, and access to public accommodations, transportation, and telecommunications.

Title II specifically applies to “public entities” (state and local governments) and the programs, services, and activities they deliver. Title II, Article 8, requires public entities to take several steps designed to achieve compliance. Federal mandates require the Transition Plan shall, at a minimum include:

1. A list of the physical barriers in a public entity’s facilities that limit the accessibility of its programs, activities, or services to individuals with disabilities.
2. A detailed outline of the methods to be utilized to remove these barriers and make the facilities accessible.
3. The schedule for taking the necessary steps to achieve compliance with Title II.
4. The name of the official responsible for the Plan’s implementation.

Transition Plans provide a method for public entities to schedule and implement ADA required improvements to existing streets and sidewalks. Before a Transition Plan can be developed, however, an inventory of the

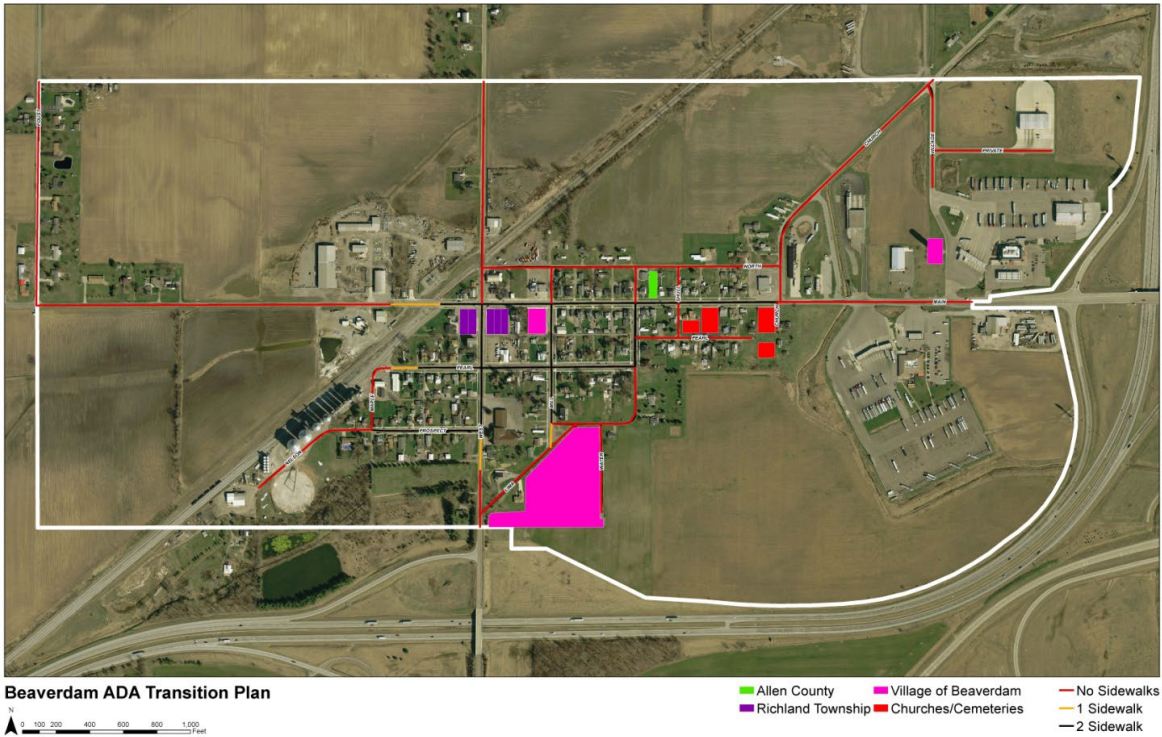
current public road rights-of-way including curb ramps and sidewalks must first be developed.

### **OBSTACLES TO THE PUBLIC RIGHTS-OF-WAY**

**People have differing abilities:** A variety of users need to access the sidewalk system. Their abilities vary in agility, balance, cognition, coordination, endurance, flexibility, hearing, problem solving, strength, vision, and pace. Pedestrians have varying needs, therefore, changing a design to enhance access for one group can create additional barriers for other individuals. The goal should be to make all sidewalks accessible to the largest possible number of residents.

To improve the pedestrian paths within the public road rights-of-way, the Village of Beaverdam worked with the Ohio Department of Transportation (ODOT) and Regional Planning Commission. The Village of Beaverdam, ODOT and the Regional Planning Commission have worked to identify available programmatic federal and state funding to help underwrite the construction of sidewalks. The Planning Commission also worked with the Village to identify barriers within the public rights-of-way including curbs, sidewalks, pedestrian crossings, and parking lots. Village officials were ultimately able to address a prioritized list of improvements based on an inventory of existing land use activities, and publicly owned buildings. Field surveys/inspections and the use of GIS mapping were employed prior to the public planning to expedite informed decision making. Of critical importance were the public rights-of-way serving government offices, downtown core areas, parks, public and quasi-public buildings and residential areas. Such data constitutes the foundation of the Transition Plan.

As a result of the inventory and the public planning process, the Village of Beaverdam has identified and assessed obstacles in the public rights-of-way. The Village adopted a proactive approach to removing barriers in the public, predicated upon public comment/complaints, and ensuring that all future construction/repair within the road rights-of-way, regardless of funding source, will in a build-out scenario, ensure compliance with ADA design standards within Village rights-of-way.



## General Policies:

### Roadway Intersections

The Village of Beaverdam will work in good faith to have curb ramps or blended intersections constructed or upgraded to achieve ADA compliance within all capital improvement projects at roadway intersections. There may be limitations which make it technically infeasible for an intersection corner to achieve full accessibility within the scope of a project. If so, those limitations will be noted and those intersection corners will remain in the Transition Plan. As future projects or opportunities arise, those roadway intersection corners shall be incorporated into future work. Regardless of whether full compliance can be achieved in all cases, each intersection corner shall be made as compliant as possible in accordance with the purpose of the ADA Transition Plan.

### Deficient Sidewalks

The Village of Beaverdam will work in good faith to have sidewalks constructed or upgraded to achieve ADA compliance within all capital improvement projects. There may be limitations which make it technically infeasible for segments of sidewalks to achieve full accessibility within the scope of a project. If so, those limitations will be noted and those segments will remain in the Transition Plan. As future projects or opportunities arise,

those segments shall continue to be incorporated into future work. Regardless of whether full compliance can be achieved in all cases, each sidewalk or trail shall be made as compliant as possible in accordance with the purpose of the ADA Transition Plan.

**BARRIER REMOVAL PRIORITIES**

The Village of Beaverdam based barrier removal priorities on three (3) factors: use, location, and relative accessibility within the public road rights-of-way. The Village identified Plan priorities as follows:

1. Rights-of-way serving government and public facilities
2. Rights-of-way serving commercial services and employment centers
3. Rights-of-way serving other land uses

**Accessibility Condition**

The Village worked to identify the relative accessibility of the public rights-of-way during the inventory phase, survey phase and public comment period.

**Priority Ranking**

In order to determine the overall priority of specific improvements to enhance accessibility within the public rights-of-way, the following criteria were used.

<b>PRIORITY OF CAPITAL IMPROVEMENTS NEEDED</b>			
<b>Land Use Type</b>	<b>Locations Serving Government Facilities</b>	<b>Commercial Services &amp; Employment Centers</b>	<b>Other Land Uses</b>
	1	2	3

The priority of improvements is based on identifying which of the public rights-of-ways serve a particular area/site within the Village, and the accessibility of the rights-of-way with respect to barriers. Appendix A identifies a prioritized list of projects by obstacle, location and 2019 estimated costs.

**Public Comment/Complaint Process**

The public comment/complaint process is fundamental and an integral part of the Transition Plan. Public complaints or requests help drive the



prioritization of improvements. To file a complaint or a request regarding accessibility of a sidewalk or curb ramp, interested parties are urged to contact the ADA Coordinator in writing and describe the issue in detail, including the location. The ADA Coordinator will inspect and document the need for further possible action. The ADA Coordinator will record the formal response and reply to the complainant/requestor. All complaints or requests will be kept on file and will include the response. Appendix B contains the Village of Beaverdam Public Comment/Complaint Form for Pedestrian Facilities in the Public Rights-of-Way.

### **New Construction & Alterations**

In order to ensure the correct design of curb ramps, sidewalks, and crosswalks in new construction and alterations within the public rights-of-way, the Village of Beaverdam will follow the Ohio Department of Transportation (ODOT) design specifications (see Appendix C). Moreover, whenever there is an alteration, intersection, roadway improvement project or new construction project, any affected curb ramp, sidewalk, and/or crosswalks within the affected area will be rebuilt to such ADA design guidelines. In order to improve accessibility the Village may choose to integrate PROWAG design standards into certain projects where feasible and reasonable. The Village will require minimum 5' sidewalk widths whenever federal MPO/ODOT/DOT monies are used in financing such improvements.

### **SCHEDULE**

As opportunity allows, the Village of Beaverdam will make efforts to improve the ADA Accessibility of pedestrian facilities in the public rights-of-way. At this time, the Village of Beaverdam has budgeted \$10,000 annually for sidewalk improvements. There is a 9-year plan for streets to be scheduled for ADA compliance to include: Main Street, West Street, North Street, Mill Street, Prospect Street, Lima Street, Water Street, Church Street, Pearl Street, Maple Street, Village Street, and North Dixie Highway. Note that this schedule is flexible, as changes may occur such as additional funding, storms, improvements due to power and electric companies, etc. There will be times when it is technically infeasible to provide technical compliance: for examples, if clear space at the top of the ramp is obstructed by a building, or the slope the approach is so extreme as to prevent a reasonable slope for a ramp in both directions. The inventory process may not account for such situations and could show a high-priority rating when all feasible actions have been taken.

Additionally, given a program as broad and comprehensive as the Village's pedestrian network, the Village will follow the concept of "program access" under Title II of the ADA. Program access does not necessarily require an entity to make every approach of each of its existing facilities accessible to and usable by individuals with disabilities, as long as the program as whole is accessible. Under this concept, the Village may choose not to install a sidewalk at some locations (or to install them as a lower priority later), as long as a reasonable path of travel is available even without a compliant sidewalk.

### **RESPONSIBLE INDIVIDUAL**

The official responsible for the implementation of the Village of Beaverdam's ADA Transition Plan for the pedestrian facilities in the public rights-of-way is:

Mr. Mike Leis  
Village Administrator & ADA Coordinator  
Phone Number: 419-643-4231  
Email Address: [mleis@midohio.twcbc.com](mailto:mleis@midohio.twcbc.com)  
Website: [www.beaverdamoh.com](http://www.beaverdamoh.com)  
Fax Number: 419-643-8473

### **PUBLIC INPUT**

The Village of Beaverdam provided opportunities for individuals to comment on this Transition Plan, which included:

- DRAFT and FINAL Document (Transition Plan) made available on the Village website.
- Public Meetings held every 1<sup>st</sup> and 3<sup>rd</sup> Tuesday in January 2019.
- Open house and presentation at a public meeting scheduled for January 15, 2019.

The Village of Beaverdam published a legal notice in the largest newspaper of general circulation, The Lima News starting on December 3, 2018. (See Appendix E) The legal notice announced the availability of the Draft ADA Transition Plan at the Village Hall with easy public access. Hardcopies of the finalized plan have been made available at the Village Hall. The Village also made the ADA Transition Plan available at the Beaverdam Fire Department during normal business hours. The Village website published notices and provides instruction regarding the timetable for comments and where to send them. Public comments were accepted for no less than 30

days and continue to be accepted today. The form for public comments/concerns is attached as Appendix B.

Formal adoption of the ADA Transition Plan is currently scheduled to take place on January 15, 2019. Thereafter, the Transition Plan will be available on the Village website and by written formal request to the ADA Coordinator.

## APPENDIX A: PUBLIC RIGHTS-OF-WAY INVENTORY



The roadway and sidewalk network in the Village of Beaverdam is dense and walkable. The existing roadway infrastructure is in good condition and sidewalks within the public rights-of-way are typically in a compliant state when present. Instances of noncompliance with ADA requirements stem from the lack of an adopted standard. The Village should adopt an ordinance regulating the construction and repair of sidewalks and driveway approaches located within the public rights-of-way. Table 1 provides recommendations to improve accessibility within the Village.

The most recurring instance of noncompliance is placement of detectable warning devices. The devices are intended to keep pedestrians on the proper path to cross intersections safely. The truncated dome mats intended to provide guidance to visually impaired pedestrians safely across intersections were found to be noncompliant.

Residents that require ADA related provisions would benefit from adding segments of sidewalk to complete pedestrian routes. Oftentimes sidewalks have not been extended through parking lots or across alleys. Crosswalks on Main Street were not provided to allow pedestrians to travel north-south safely.

The Beaverdam Village Park is not served by sidewalk and no accommodations for residents with disabilities have been afforded. The area currently used for parking is gravel and therefore not wheelchair accessible. Park sidewalks warrant the consideration and prioritization over other stretches in the Village awaiting improvements.

Table of Proposed Improvements				
Location	Deficiency	Improvement	Cost Estimate	Priority Tier
Main St. & Mill St.	Town Hall does not have sidewalk through parking lot.	Complete sidewalk through Town Hall parking Lot. Include alley crossings to the South and a Crossing on Main Street.	3,000	1
Village Park	Park is not accessible by sidewalk.	Continue sidewalk from terminus on east side of Mill. Extend south via crosswalk and wrap around park to shelter house.	17,500	1
Main St. & West St.	There are no crosswalks for pedestrians to move safely through the Village's busiest intersection. Two poles serving as barriers in ramp. Fire Dept. requires handicap parking.	Add crosswalks to N, S, E leg of intersections. Complete sidewalk through Fire Dept. lot and add ADA space. Install new ramps in all 4 corners (remove barriers in SE) to support N-S and E-W movement.	16,500	1
Main St. @ Commercial Area	The northern sidewalk does not extend through lots in east side of town (Waffle House, Dollar General, etc).	Connect sidewalks from Waffle House to Pilot driveway.	19,000	1
Lima St. & Pearl St. (North)	Sidewalk is missing segment through alley west of Lima St.	Complete side walk through alley. Correct detectable warnings and in turn replace crosswalk striping.	2,000	2
Lima St. & Pearl St. (South)	NW corner has incorrectly placed warning device.	Correct detectable warnings and in turn replace crosswalk striping. If R/W is not necessary add an east landing for the southern crossing.	2,000	2
Prospect St. & West St.	West St does not allow pedestrians to cross safely.	Add a crosswalk to south side of Prospect. Build out SW corner with proper sidewalk landing and warning devices.	1,500	2
Church St. & Main St.	Pedestrian traffic cannot continue eastward on south side of Main St.	Add a crosswalk to Church St. and Main St. (West of Church). Complete sidewalk through Thirsty's lot. Install ramp and appropriate warning devices at the NW & SW corners. Correct the NE corner that receives the foot traffic from Thirsty's.	7,500	2
West St.	Two alleys south of Main St. are obstructions to sidewalk wheelchair accessibility.	Extend sidewalk through both alleys.	2,000	2
Pearl St. & West St.	NW & SE corners do not have detectable warning devices.	Install two properly placed warning devices at each corner.	7,500	3
Pearl St. & West St.	NE & SW corners have incorrectly placed warning devices.	Remove misplaced warning devices and install two devices at each corner with proper placement.		3

Mill St. & Pearl St.	NW, SE, NE corners have incorrectly placed warning devices.	Remove misplaced warning devices and install two devices at each corner with proper placement.	5,000	3
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**APPENDIX B:  
PUBLIC COMMENT/CONCERN  
FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHTS-OF-WAY**

In an attempt to comply with the American with Disabilities Act, Mayor Pamela LePine and Mike Leis, Coordinator of the Village of Beaverdam ADA Transition Plan are soliciting public comments and concerns regarding the ADA Transition Plan and pedestrian facilities within the public rights-of-way. Residents are encouraged to participate in the identification of obstacles and the ongoing improvement of public rights-of-ways within the Village of Beaverdam.

Mike Leis can be reached for comment by telephone at 419-643-4231, by email at [mleis@midohio.twcbc.com](mailto:mleis@midohio.twcbc.com), or fax 419-643-8473. Grievance procedures will require the filing of this form available at: [www.beaverdamoh.com](http://www.beaverdamoh.com) and at the Beaverdam Village Hall, 101 W Main Street, Beaverdam, Ohio 45808.

**PUBLIC COMMENT/CONCERN & RESPONSE FORM**  
(Please provide specific locations and or pictures of obstacles)

**Date of Comments:** \_\_\_\_\_

**Name of Person:** \_\_\_\_\_

**Comments:** \_\_\_\_\_

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**Response:** \_\_\_\_\_

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# APPENDIX C: THE OHIO DEPARTMENT OF TRANSPORTATION (ODOT) DESIGN SPECIFICATIONS

REVISION DATE 7-20-2018	STATE OF OHIO TRANSPORTATION ADMINISTRATION David L. Holstein ENGINEER	D. Fisher ENGINEER	OFFICE OF ROADWAY ENGINEERING (with Detectable Warnings) NEW CURB RAMPS STANDARD DRAWING CONSULTATION DRAWING
THIS DRAWING REPLACES BP-7.1 DATED 7-18-14.			
<p>Construct each curb ramp using Type A1 details on Sheet 2.</p>	<p>Use curb ramps with returned curbs where buffer is wide enough to accommodate ramp slope.</p>	<p>Construct each curb ramp using Type A2 details on Sheet 2.</p>	<p>Use curb ramps with flared sides at locations with wide sidewalks.</p>
<b>PERPENDICULAR CURB RAMPS</b>			
<b>PREFERRED CONSTRUCTION PLACEMENT</b>			
<b>PARALLEL CURB RAMPS</b>			
<b>COMBINATION CURB RAMPS</b>			
<b>DIAGONAL RAMP (Type D)</b>			
<b>PERPENDICULAR RAMPS</b>			
<b>ACCEPTABLE CONSTRUCTION PLACEMENT</b>			
<b>NOTES</b>			
<p><b>GENERAL:</b> This drawing shows curb ramp types, details and placement examples for curb ramp construction, including the installation of detectable warnings. Curb ramp types are shown on Sheet 2 and include Perpendicular, Parallel, and Combined types as specified to be constructed in the locations shown on the project plans.</p> <p>Curb ramps added to an existing intersection or walk should be individually detailed on the drawings and include the detectable warning strip. The contractor may adjust the placement of curb ramps if existing field conditions warrant with the approval of the Engineer.</p> <p><b>PAYMENT:</b> Measure and pay for the ramp area within the shaded limits of this drawing as Item 509 Curb Ramp, Square Foot. This includes the cost of any curb or curb and gutter, forming, and finishing required within the shaded area.</p> <p>Work to be used the shaded ramp/finish area is paid for as curb (600) and walk (600). Removal of existing curb, walk (or existing curb ramps) are paid under Item 202.</p> <p>For at-grade crossing locations where only detectable warnings are required in order to achieve ADA compliance, measure and pay for the strip of detectable warnings as Item 509 Detectable Warning, Square Foot. The work to cost the files in place will also include the cost of existing pavement (Item 202) to the nearest joint, or if no joint exists, a minimum of 4 feet.</p>			

**NOTES CONTINUED**

The running slope of the curb ramp shall be a 1:21 maximum or flatter. In existing sidewalks, where the maximum ramp slope is not feasible due to utility poles or vaults, right-of-way limits it may be reduced as follows:

- a) 1:24 for a max. rise of 5'.
- b) 1:21 over a max. run of 2'-0" for historic areas where a flatter slope is not feasible.
- c) 1:21 over a max. run of 2'-0" for historic areas where a flatter slope is not feasible.

To prevent closing the grade inactively, the transition from existing sidewalk to the shaded curb ramp area is not required to exceed 16 feet in length.

While ramps may be sloped to the crosswalk, the entire lower landing area shall be paved with a material that meets ADA requirements and cannot be located in the traveled lane of opposing traffic.

The bottom edge of the ramp shall change planes perpendicular to the landing. The bottom edge of the gutter on street at the foot of a curb ramp, landing, or sloped transition shall be 20" or flatter.

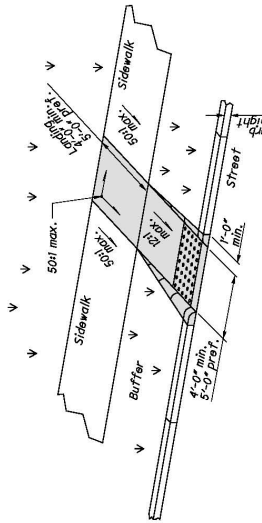
The edge of the curb shall be flush with the edge of the adjacent pavement and gutter and surface slopes that meet grade breaks shall also be flush. Ramp landings shall be 4' min. x 4' min. with a 50:1 or flatter cross slope and running slope.

**DETECTABLE WARNINGS:** Install detectable warnings on each curb ramp with approved materials, as shown on Sheet 3. Install these proprietary products as per manufacturer's written instructions.

**DRAINAGE:** Contractor is to ensure the base of each constructed curb ramp allows for proper drainage, without exceeding allowable cross slope or ramp gutter, and 2) gutter and ramp, are not allowed.

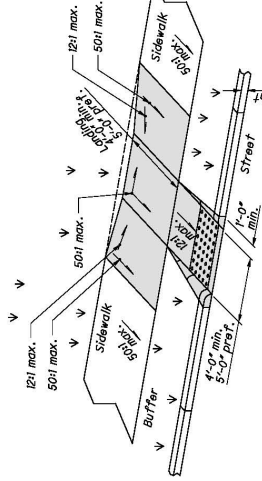
**SURFACE TEXTURE:** Texture concrete surfaces by coarse brooming transverse to the ramp slopes to be rougher than the adjacent walk.

**JOINTS:** Provide expansion joints in the curb ramp as extensions of walk joints and consistent with Item 603.03 requirements for a new concrete walk. Provide a 1/2" Item 705.03 expansion joint filler around the edge of ramps built in existing concrete sidewalks. Provide expansion joints between the ramp edges and slope changes, and do not necessarily indicate joint lines.



Type A1 (Perpendicular with flared sides)

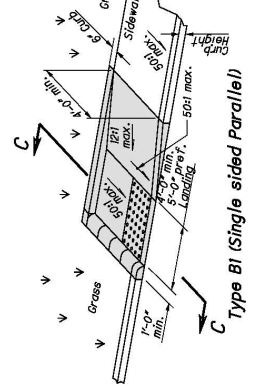
**PERPENDICULAR CURB RAMP DETAILS**



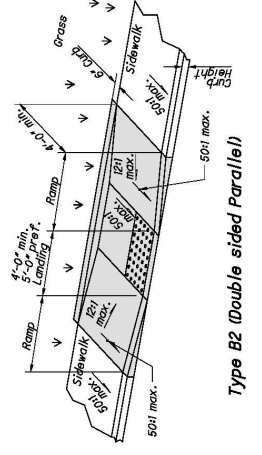
Type A2 (Perpendicular with returned curb)

**COMBINED CURB RAMP DETAILS**

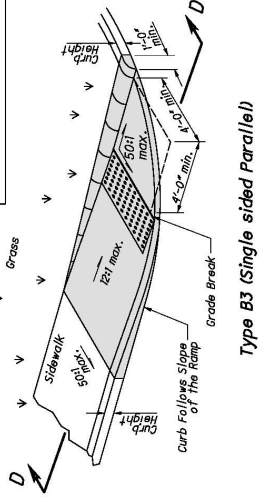
**COMBINED CURB RAMP DETAILS**



Type B1 (Single sided Parallel)



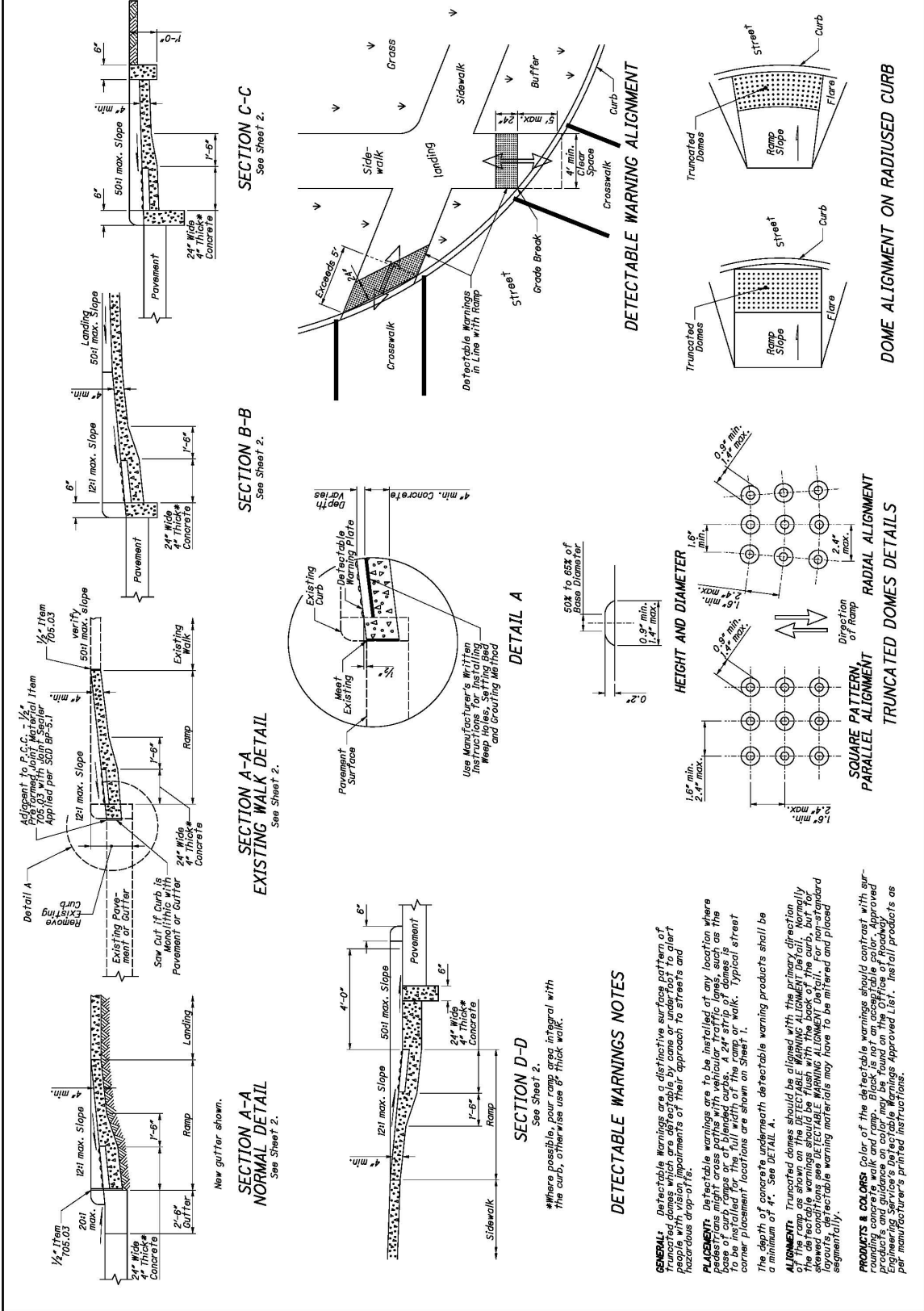
Type B2 (Double sided Parallel)



Type B3 (Single sided Parallel)

See Sheet 3 for Sections.

THIS DRAWING REPLACES BP-7.1 DATED 7-18-14.



**APPENDIX D:  
VILLAGE OF BEAVERDAM POLICY STATEMENT GOVERNING  
THE AMERICANS WITH DISABILITIES ACT TRANSITION PLAN  
& PEDESTRIAN FACILITIES IN THE PUBLIC RIGHTS-OF-WAY**

**Introduction:**

The purpose of this Plan is to ensure that the Village of Beaverdam creates reasonable accessible paths of travel in the public rights-of-way for everyone, including people with disabilities. The Village of Beaverdam is making a significant and long-term commitment to improving the accessibility of pedestrian facilities. The Transition Plan will identify physical barriers and prioritize improvements that should be made throughout the Village of Beaverdam. This Transition Plan will describe the existing policies and programs to enhance the overall pedestrian accessibility.

**Legal Requirements:**

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Title II specifically applies to “public entities” (state and local governments) and the programs, services and activities they deliver. Title II, Article 8, requires public entities to take several steps designed to achieve compliance. The Village Transition Plan will, at a minimum include:

- A list of physical barriers in a public entity’s facilities that limit accessibility of its programs, activities, or services to individuals with disabilities.
- A detailed outline of the methods to be utilized to remove these barriers and make the facilities accessible.
- The schedule for taking the necessary steps to achieve compliance with Title II.
- The name of the official responsible for the plan’s implementation.

The Village is in an ongoing process of identifying and assessing obstacles in the public rights-of-way through the use of an Inventory Tool. Barrier removal priorities based on location land use and accessibility conditions are integrated within the inventory process. It is very important that the

public be aware that their involvement in this Plan is a valuable component to Village efforts toward establishing a workable plan of compliance.

**Public Complaint:**

Public complaints, suggestions and comments are an integral part of the Transition Plan. Public comments may often drive the prioritization of improvements. To file a comment, concern or request, please mail the Village of Beaverdam at 101 W Main Street, Beaverdam, Ohio 45808 or by telephone at 419-643-4231, by email at [mleis@midohio.twcbc.com](mailto:mleis@midohio.twcbc.com), or fax 419-643-8473.

**Public Input:**

Public Comment & Response Forms are available on the Village Website at the following link: <http://www.beaverdamoh.com>. The Village of Beaverdam welcomes your participation and unique outlook as to how the Village can meet and exceed ADA requirements and recommendations. Public involvement in the Village ADA plan has been a valuable component to the Village in compiling w workable plan of compliance. We continue to solicit your additions to the ADA Transition Plan.

**Summation:**

An inventory of the Village of Beaverdam streets is included with this Transition Plan. The inventory reflects a prioritized list of projects to necessary to complete ADA requirements. An annual appropriation to bring sidewalks into ADA compliance will work to eliminate barriers and complete the requirements as the Village authorizes paving projects. The Village will annually reassess priorities to better serve the mobility limited and work to ensure reasonable access and ADA compliance within the community.

**Pamela LePine, Mayor**

## **APPENDIX E: PUBLIC NOTICE**

Please take notice that the Village of Beaverdam is in the process of compiling an Americans with Disabilities Act (ADA) Transition Plan. The Village of Beaverdam invites the public to become involved in this process through written comments or attendance at Village Council meetings, held the first and third Tuesday of each month at 7:00 PM at the Village Hall, located at 101 W Main Street, Beaverdam, Ohio 45808.

The Mayor has appointed Village Administrator Mike Leis, as Coordinator of the Village of Beaverdam ADA Transition Plan for pedestrian facilities in the public rights-of-way. To file a complaint, request, or offer suggestions regarding accessibility of a sidewalk or curb ramp please contact the ADA Coordinator who will inspect and document the concern for possible action. The Coordinator will subsequently respond to the complainant/requestor within 30 days. Mr. Leis can be reached at the by telephone at 419-643-4231, by email at [mleis@midohio.twcbc.com](mailto:mleis@midohio.twcbc.com), or fax 419-643-8473.